

Maintenance Rating Program

Triangle Expressway

February 2026

2025 Fourth Quarter Report

CONSULTANT CERTIFICATION OF COMPLETION

February 10, 2026

Alan Shapiro, P.E.
Deputy Chief Engineer
North Carolina Turnpike Authority
2501 Aerial Center Parkway, Suite 200
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NCTA Triangle Expressway Roadway Maintenance Performance Rating Program; Q4, 2025 Rating

This is to certify that I, Adam Gosselin, PE, am an authorized official representative of the company Mott MacDonald I&E, LLC, a subconsultant to HNTB North Carolina, P.C. Collaboratively; we are working as the NCTA Roadway and Facility Maintenance Performance Rating Program Consultants.

I know of my own personal knowledge, and do hereby certify, that the work of the contract described above has been independently performed in accordance with, and in conformity to, the *NCTA Roadway and Facility Maintenance Performance Standards v.7.1*.

Sincerely,

Mott MacDonald I&E, LLC



Adam Gosselin, PE
Principal for Mott MacDonald
PE #038213

930 Main Campus drive, Suite 200
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1.0 Executive Summary

The North Carolina Turnpike Authority (NCTA) Maintenance Rating Program (MRP) is a maintenance evaluation program for roadway features and toll facilities on the NCTA system. This report presents results from the 2025 Fourth Quarter Assessment of the Triangle Expressway.

The overall 2025 fourth quarter maintenance rating of the Triangle Expressway was **96.4**, above the NCTA target rating of 90. As shown in **Table 1**, all five elements assessed achieved a rating greater than the target rating of 85.

Table 1: MRP Element Results for the 2025 Fourth Quarter Assessment

Element	MRP Rating	Target Rating
Road Surface	100.0	85.0
Unpaved Shoulders and Ditches	100.0	85.0
Drainage	96.1	85.0
Roadside	98.6	85.0
Traffic Control Devices	90.7	85.0
Overall MRP Performance Rating	96.4	90.0

This report also provides a rolling rating of the latest four quarterly inspections of the Triangle Expressway. As presented in **Table 2**, the rolling maintenance rating of the Triangle Expressway was **96.2**.

Table 2: MRP Rolling Element Results

Element	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Road Surface	100.0	96.0	100.0	100.0	99.1
Unpaved Shoulders and Ditches	100.0	100.0	100.0	100.0	100.0
Drainage	95.6	96.9	94.6	96.1	95.8
Roadside	99.1	97.7	99.2	98.6	98.6
Traffic Control Devices	89.5	89.3	94.5	90.7	91.0
Overall MRP Performance Rating	96.0	94.9	97.4	96.4	96.2

In addition, the report provides findings of the Green Level Historic District signs inspection. This quarter, two signs were inspected. Both signs were in good physical condition, with one being recently re-erected. The landscaped area around one of the signs was maintained in accordance with NCTA MRP standards, while the landscaping bed around the recently erected sign needs attention.

2.0 Introduction

The NCTA MRP is a comprehensive planning, measuring, and managing process that provides a means for communicating to managers, stakeholders, and customers the impacts of policy and budget decisions on program service delivery.

Using outcome-based performance measures and the service level scale (0 through 100), the inspection results are rated against established threshold criteria. The program analysis is accomplished using sampling procedures that capture the level of service being provided for individual assets. The evaluation procedure is based on the establishment of threshold conditions that quantify the maximum defect allowed on assets. Over time, the results can be charted to identify work needs and subsequent necessary actions.

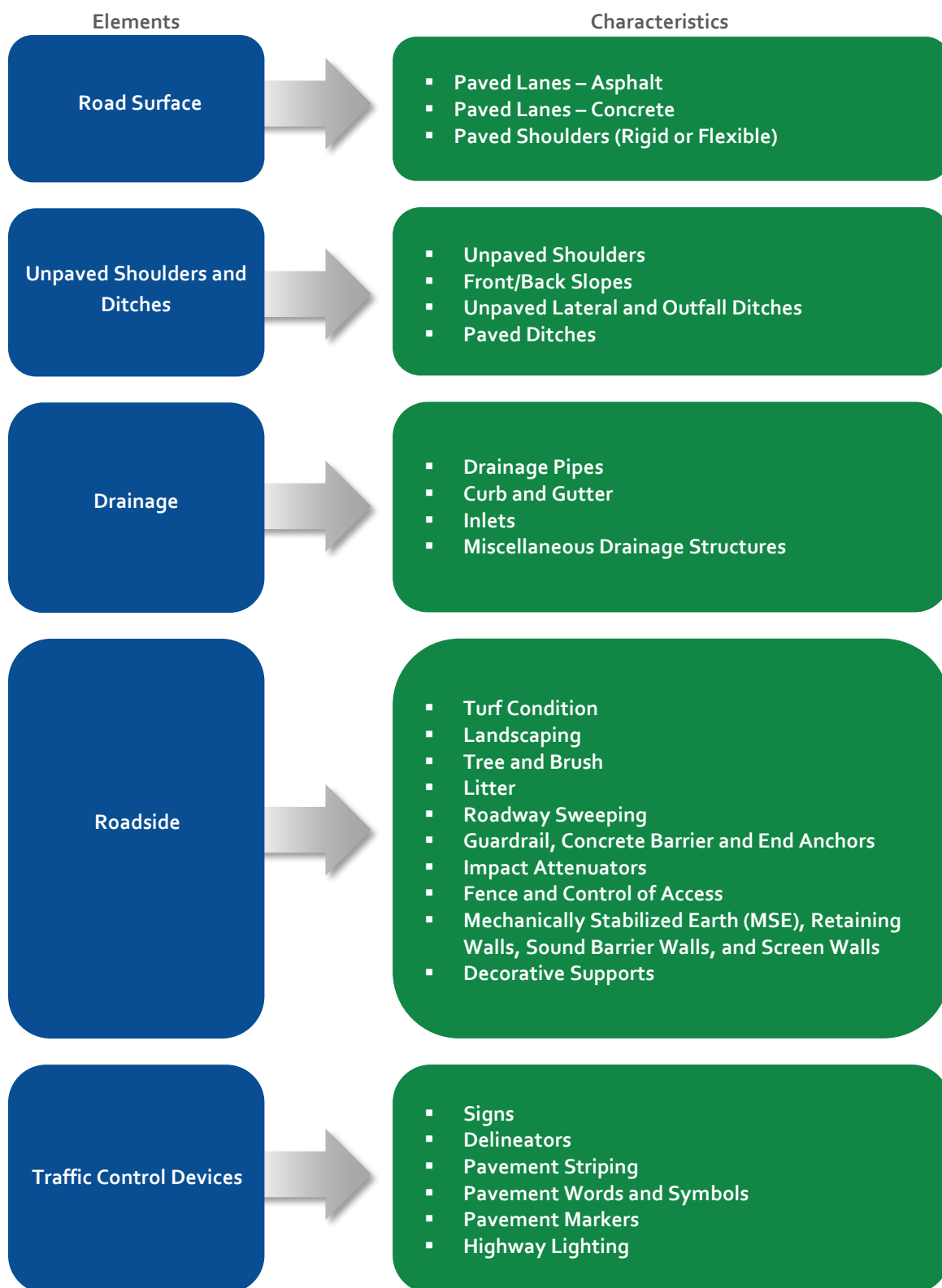
The NCTA performance standards, threshold criteria, and maintenance rating program were developed through a collaborative effort by NCTA managers, NCDOT maintenance staff, and consultants.

Using field survey information, a maintenance matrix can be developed to show the ties between maintenance activities and the characteristics of various roadway features. The purpose of this evaluation is to provide information that can be used to schedule and prioritize routine maintenance activities and provide uniform maintenance conditions that meet established objectives.

3.0 MRP Procedure

Per the *NCTA Roadway and Facility Maintenance Performance Standards V7*, roadway assets or characteristics on NCTA facilities have been grouped into elements. These elements and corresponding characteristics are shown in **Figure 1**:

Figure 1: Maintenance Elements and Characteristics



A weighting system has been established to identify the importance of each element and characteristic. This system consists of two weighting factors: one that accounts for the importance of individual characteristics within a given maintenance element (1-9), and one that accounts for the importance of the maintenance elements to the total rating (by % of score). This two-factor system reveals deficiencies among characteristics and elements.

The program analysis is accomplished using statistically valid, random sampling procedures that capture the level of service for individual characteristics, with a 95% confidence level in sampling. The sample characteristics selected are evaluated during quarterly inspections, which are performed during the months of February, May, August, and November to account for dynamic changes in assets during the various seasons. The evaluation process is completed using electronic data collection tablets and is based on established threshold conditions described in the NCTA Roadway and Facility Maintenance Standards V7. Those characteristics that meet or exceed the threshold are coded as PASSING; those that do not meet the threshold are coded as NOT PASSING.

When the evaluation process is completed, the number of PASSING samples and total sample are multiplied by the weighted values (1-9) to determine the actual and possible rating points for characteristics and elements. MRP ratings for elements and characteristics are then calculated as the ratio of the actual rating points to possible rating points. The MRP ratings represent the maintenance level of service currently being provided, as they define the percent of characteristics and elements that meet the maintenance condition standard. For instance, an MRP rating of 83 signifies that 83 percent of the inspected elements/characteristics met the standard.

The overall MRP rating is determined by calculating the sum of the elements' ratings multiplied by the following weighted factors:

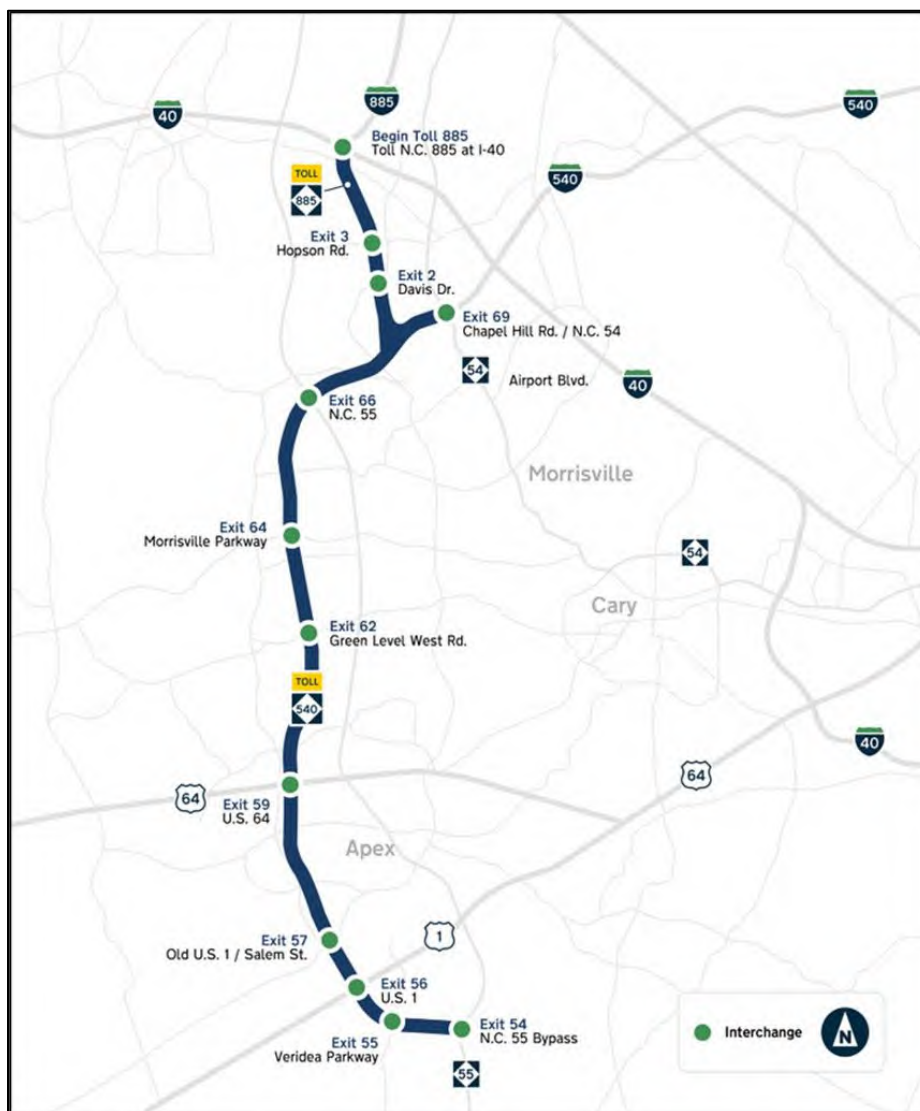
Road Surface =	25%
Unpaved Shoulders =	13%
Drainage =	15%
Roadside =	17%
Traffic Control Devices =	30%
<hr/>	
Total	100%

The NCTA's overall target rating is 90, with elements scoring 85 or higher, and characteristics 80 or higher. In addition to quarterly ratings, the cumulative rolling annual rating is calculated each quarter. This rating is obtained by adding the ratings of the latest four quarterly inspections to compensate for the likelihood of uneven sample sizes.

4.0 Triangle Expressway Description

The Triangle Expressway extends for approximately 37 miles from the interchange of I-40 and Toll NC-885 in Durham to an easternly connection with I-40 / I-42 near Garner. It includes a one-mile segment on Toll NC-540 extending north from the Toll NC-540 / Toll NC-885 interchange to the NC-54 interchange. The Triangle Expressway consists of eighteen interchanges and thirty-four all-electronic toll collection zones. The newest section from N.C. 55 Bypass to I-40/I-42 near Garner opened to traffic and started toll collection on September 25, 2024. This section includes interchanges at Holly Springs Road, Bells Lake Road, U.S. 401, Old Stage Road, and N.C. 50 before connecting with I-40 and I-42. While the newly opened extension is now open to traffic, this report will only summarize the data for the pre-existing roadway segment from Exit 54 north/west. (Figure 2).

Figure 2: Triangle Expressway Map



5.0 Triangle Expressway Asset Inventory Update

Through normal day-to-day maintenance activities and the construction of special projects, roadside assets are continuously being added or modified on the roadway. NCTA coordinates closely with NCDOT Division 5 Maintenance and conducts routine field visits to maintain an accurate asset inventory and ensure the validity of the MRP survey.

Prior to this quarter assets on Toll NC 540 exit ramps to and from NC-55 Bypass were removed from the inventory due to the Complete 540 construction project. **Table 3** presents the updated number of assets that are currently available for inspections.

Table 3: Asset Inventory

Assets	Total Inventory	2025 Eligible Inventory
Barriers	801	616
Curb and Gutter	428	391
Decorative Supports	305	298
Drainage	1179	1127
Misc. Drainage	218	202
Fences	508	483
Highway Lighting	435	430
Impact Attenuators	48	46
Inlets	1129	1075
Linear Segments	795	755
Plant Beds	266	257
Paved Ditches	2	2
Pavement Symbols	652	591
Signs	1224	1168
Tree and Brush	603	567
Turf	1074	1011
Walls	88	84

6.0 MRP Fourth Quarter Assessment

6.1 Quarterly Results

The overall 2025 fourth quarter maintenance rating of the Triangle Expressway was 96.4, above NCTA's target overall rating of 90. All elements assessed achieved quarter ratings above the target rating of 85 established for element groups.

It is important to note that these results are only representative of the fourth quarter sample, one of the four surveys to provide an intermediate snapshot of seasonal conditions. Therefore, they are not a statistically valid representation of the assets; only the total of all four quarterly inspections, reported as the rolling rating, provides a 95% confidence level in statistical sampling. The fourth quarter MRP performance ratings for elements and characteristics are presented in *Table 4* and *Table 5*, respectively.

Table 4: MRP Element Results for Q4 2025

Element	MRP Rating
Road Surface	100.0
Unpaved Shoulders and Ditches	100.0
Drainage	96.1
Roadside	98.6
Traffic Control Devices	90.7
Overall MRP Performance Rating	96.4

Table 5: MRP Characteristics Results for Q4 2025

Road Surface	Sample Passed	Sample Total	Weighted Values	Actual Pts	Available Pts	Q4 Rating
Paved Lanes Asphalt	21	21	9	189	189	100
Paved Lanes Concrete	11	11	9	99	99	100
Paved Shoulder	32	32	5	160	160	100
Element Total				448	448	100.0
Unpaved Shoulders and Ditches	Sample Passed	Sample Total	Weighted Values	Actual Pts	Available Pts	Q4 Rating
Unpaved Shoulder	32	32	9	288	288	100
Front/Back Slopes	32	32	6	192	192	100
Lateral and Outfall Ditches, Unpaved	32	32	6	192	192	100
Ditches, Paved	2	2	5	10	10	100
Element Total				682	682	100.0
Drainage	Sample Passed	Sample Total	Weighted Values	Actual Pts	Available Pts	Q4 Rating
Drainage Pipes	32	34	7	224	238	94
Curb and Gutter	27	28	6	162	168	96
Inlets	38	39	7	266	273	97
Misc. Drainage Structure	28	29	4	112	116	97
Element Total				764	795	96.1
Roadside	Sample Passed	Sample Total	Weighted Values	Actual Pts	Available Pts	Q4 Rating
Turf Condition	32	33	7	224	231	97
Landscaping	23	25	4	92	100	92
Trees and Brush	31	31	4	124	124	100
Litter	32	32	4	128	128	100
Roadway Sweeping	32	32	5	160	160	100
Guardrail, Concrete Barrier, and End Anchors	31	31	9	279	279	100
Impact Attenuators	8	9	9	72	81	89
Fence, Control Access	30	30	7	210	210	100
Retaining Walls and Sound Barrier Walls	14	14	5	70	70	100
Decorative Supports	26	26	5	130	130	100
Graffiti and Stain Removal	44	44	4	176	176	100
Element Total				1665	1689	98.6
Traffic Control Devices	Sample Passed	Sample Total	Weighted Values	Actual Pts	Available Pts	Q4 Rating
Signs	33	34	7	231	238	97
Delineators	31	32	3	93	96	97
Pavement Striping/Marking	31	32	8	248	256	97
Words and Symbols	27	31	7	189	217	87
Pavement Markers	31	32	9	279	288	97
Highway Lighting	34	46	6	204	276	74
Element Total				1244	1371	90.7

Additionally, **Appendix A** includes maps that present the location of all assets assessed during the fourth quarter. **Appendix B** includes a list of the individual assets that did not achieve their target ratings.

6.2 Quarterly Analysis and Recommendations

Elements

During the fourth quarter, all elements exceeded NCTA's quarter score threshold criteria of 85.

Road Surface (100.0) maintained a good representation from the previous quarter's rating.

Unpaved Shoulders and Ditches (100.0) elemental rating was on par with the previous quarter's rating. All characteristics within this element continued scoring above 90.

Drainage (96.1) rating increased by 1.5 points from the previous quarter rating. Misc. Drainage Structures (97) rating increased 14 points from last quarter.

Roadside (98.6) rating decreased by 0.6 points from the previous quarter rating. While most characteristics continued to score strongly, Impact Attenuators (89) declined with a score decrease of 11.0 points from the previous quarter's rating.

Traffic Control Devices (90.7) experienced an decrease in rating of 3.8 points from the previous quarter. Highway Lighting (74) decreased by 3 points in rating and will require attention with an element characteristic score below target of 80. Word and Symbols (87) dropped with an decrease in rating of 6 points.

Recommendations to improve specific critical characteristic ratings are provided in the following sections.

Characteristics

This quarter, all but one element characteristic, Highway Lighting (74), exceeded NCTA's quarter score threshold criteria of 80. A description of the characteristic's conditions and future work planning recommendations are provided below. Pictures of applicable characteristic failures are included in **Appendix B**.

Highway Lighting (74 rating – 34 of the 46 assets passed): Each of the highway lights that did not pass inspection were not functioning properly (out at night).

To maintain/improve the condition ratings, it is recommended that highway lighting be regularly inspected for functionality after dark, and maintenance completed as planned in the capital budget. Solar and battery powered backup may help cover temporary down time between repairs.

Maintenance Program:

- 1) Perform night patrol once a month and identify any outages. A monthly "Lighting Outage Report" shall be submitted by the maintenance provider to the NCTA by the 30th of each month. All bulb outages must be replaced within 48 hours.
- 2) Perform cleaning of glassware at the same time as any routine maintenance function or diagnostic action is performed.
- 3) Replace any light poles damaged by traffic within 5 days or within 14 days if any foundations needed pouring.

Maintenance and Evaluation Standards: Highway Lighting does not meet the maintenance standards when any of the following criteria is observed:

- 1) Any electrical inspection plate, access panel cover, exposed electrical wire or pull box cover are not properly secured in place.
- 2) The luminaries are not functioning during nighttime observation. (N)
- 3) Any pole is damaged, leaning or missing.
- 4) Rodent screen protection is not in place, where applicable.

7.0 Annual Results

The 2025 annual rolling maintenance rating of the Triangle Expressway was **96.2**, exceeding NCTA's target overall rating of 90. All elements exceeded NCTA's rolling rating threshold criteria of 85. Twenty-seven of the twenty-eight characteristic ratings met or exceeded the target rating of 80.

The 2025 results are presented in **Exhibit 1** and **Table 6**. These results are a collection of the four quarterly inspections conducted in the last year.

Exhibit 1: MRP Element Results for 2025

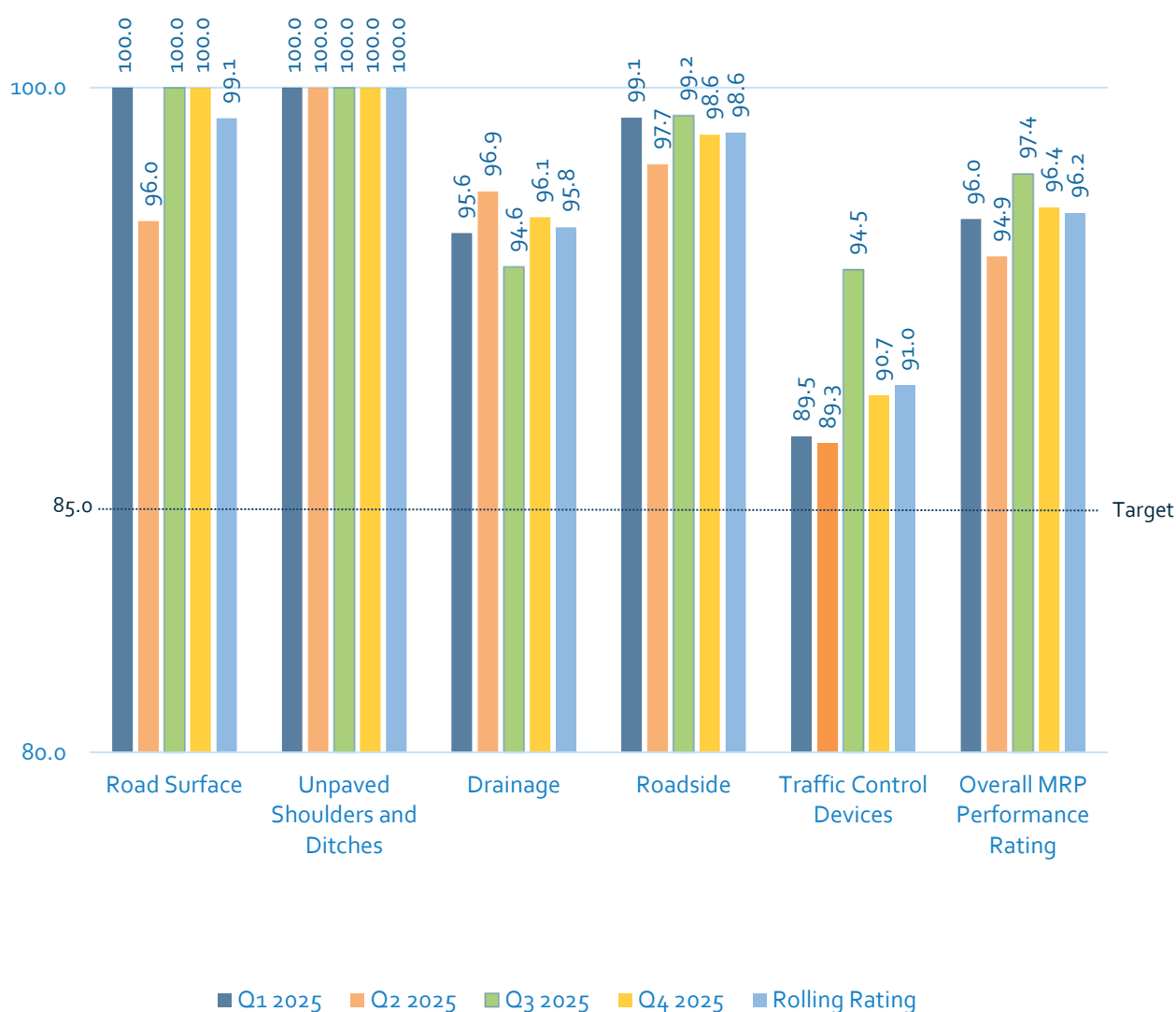


Table 6: MRP Rolling Element Results

Road Surface	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Paved Lanes Asphalt	100	86	100	100	97
Paved Lanes Concrete	100	100	100	100	100
Paved Shoulder	100	100	100	100	100
Element Total	100.0	96.0	100.0	100.0	99.1
Unpaved Shoulders and Ditches	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Unpaved Shoulder	100	100	100	100	100
Front/Back Slopes	100	100	100	100	100
Lateral and Outfall Ditches, Unpaved	100	100	100	100	100
Ditches, Paved	100	100	100	100	100
Element Total	100.0	100.0	100.0	100.0	100.0
Drainage	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Drainage Pipes	97	100	94	94	96
Curb and Gutter	90	100	100	96	97
Inlets	100	97	97	97	98
Misc. Drainage Structure	91	83	83	97	88
Element Total	95.6	96.9	94.6	96.1	95.8
Roadside	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Turf Condition	97	88	97	97	95
Landscaping	100	100	96	92	97
Trees and Brush	100	97	97	100	98
Litter	100	100	100	100	100
Roadway Sweeping	100	100	100	100	100
Guardrail, Concrete Barrier, and End Anchors	100	100	100	100	100
Impact Attenuators	90	100	100	89	95
Fence, Control Access	100	97	100	100	99
Retaining Walls and Sound Barrier Walls	100	100	100	100	100
Decorative Supports	100	100	100	100	100
Graffiti and Stain Removal	100	100	100	100	100
Element Total	99.1	97.7	99.2	98.6	98.6
Traffic Control Devices	Q1 2025 Rating	Q2 2025 Rating	Q3 2025 Rating	Q4 2025 Rating	Rolling Rating
Signs	97	100	100	97	99
Delineators	100	97	100	97	99
Pavement Striping/Marking	97	91	100	97	96
Words and Symbols	81	83	93	87	86
Pavement Markers	92	97	100	97	96
Highway Lighting	77	76	77	74	76
Element Total	89.5	89.3	94.5	90.7	91.0

7.1 Green Level Historic District Signs

Green Level Historic District signs and surrounding landscaped areas were installed as part of the Triangle Expressway construction project. Currently, NCDOT is maintaining the Green Level Historic District Signs and the Town of Cary is providing maintenance to the landscaped areas surrounding these signs.

As part of each quarterly inspection, an assessment team visits the two remaining Green Level Historic District signs to conduct a visual inspection of each sign and ensure they are in good standing. Both two signs included in the inspection inventory were found to be in good condition, while one of the landscaping beds around a recently re-erected sign needs attention. *Figure 4* shows the two areas assessed.

Figure 4: Green Level West Historic District Signs, Landscape Areas



8.0 Conclusion

This report presents the 2025 fourth quarter assessment of the Triangle Expressway. The NCTA's target ratings are 90 for the rolling rating, 90 for the overall quarter rating, 85 for elements, and 80 for characteristics. The fourth quarter 2025 overall rating was **96.4** and the rolling rating was **96.2**, both ratings met the target rating of 90.

All element ratings were above the target ratings for the quarter and rolling assessment. During the fourth quarter assessment, all but one characteristic met or exceeded the target rating of 80. The characteristic that received a quarter score less than 80 was Highway Lighting (74). To maintain/improve the condition ratings, it is recommended that highway lighting be routinely inspected for functionality after dark along with routine maintenance being completed as planned in the capital budget.

This quarter, both two Green Level Historic District signs inspected were found to be in good condition. The landscaped area surrounding the recently erected sign needs attention, while the other has been well maintained.

Appendix A: Triangle Expressway 2025 Fourth Quarter Asset Assessment Locations



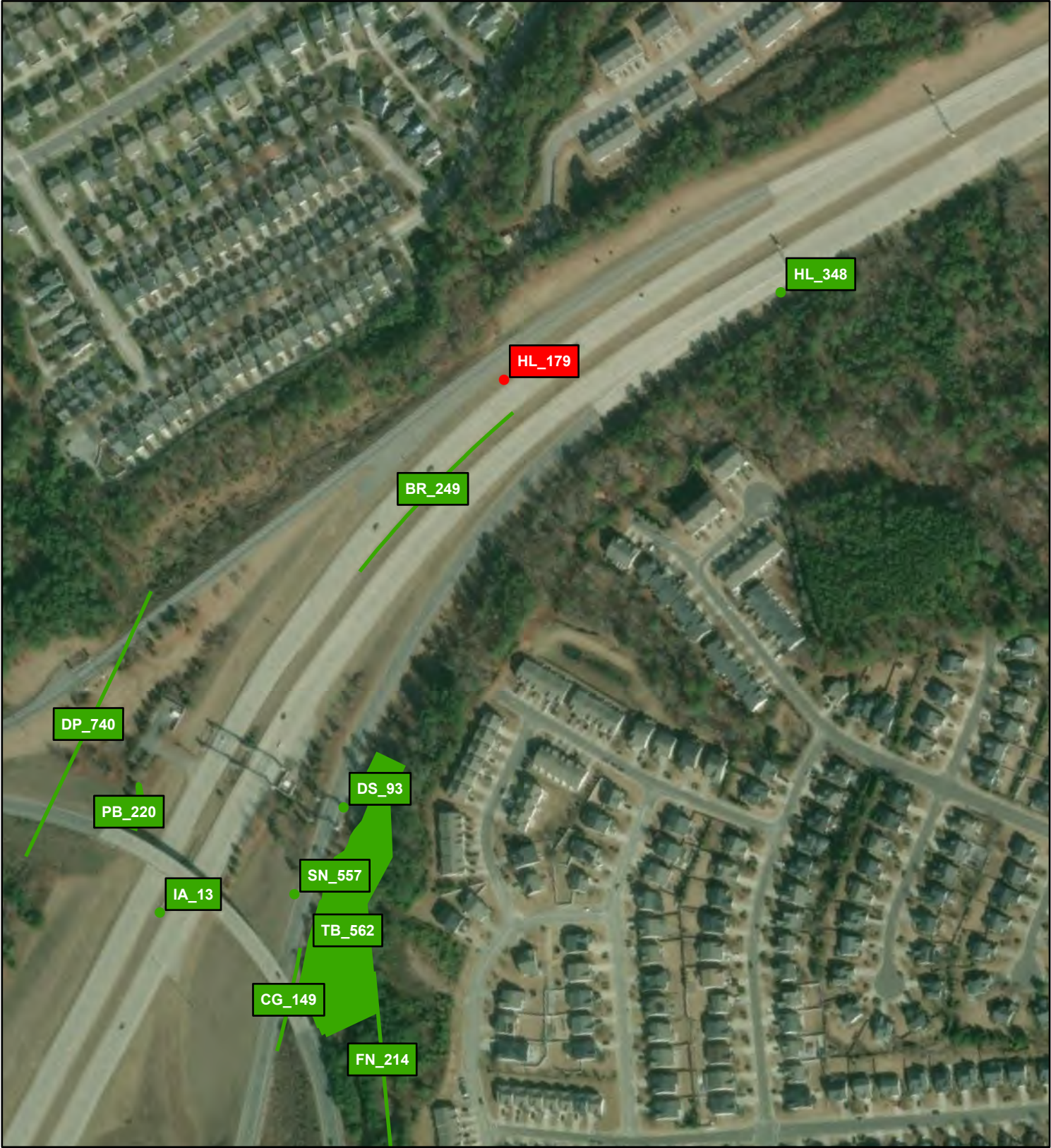
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Appendix A: Triangle Expressway 2025 Fourth Quarter Asset Assessment Locations



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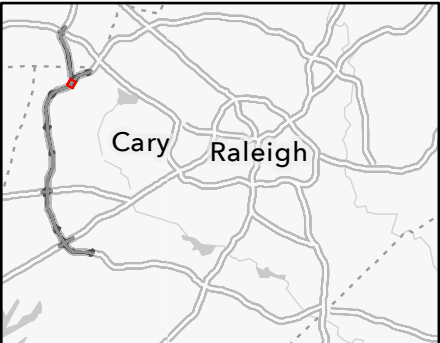
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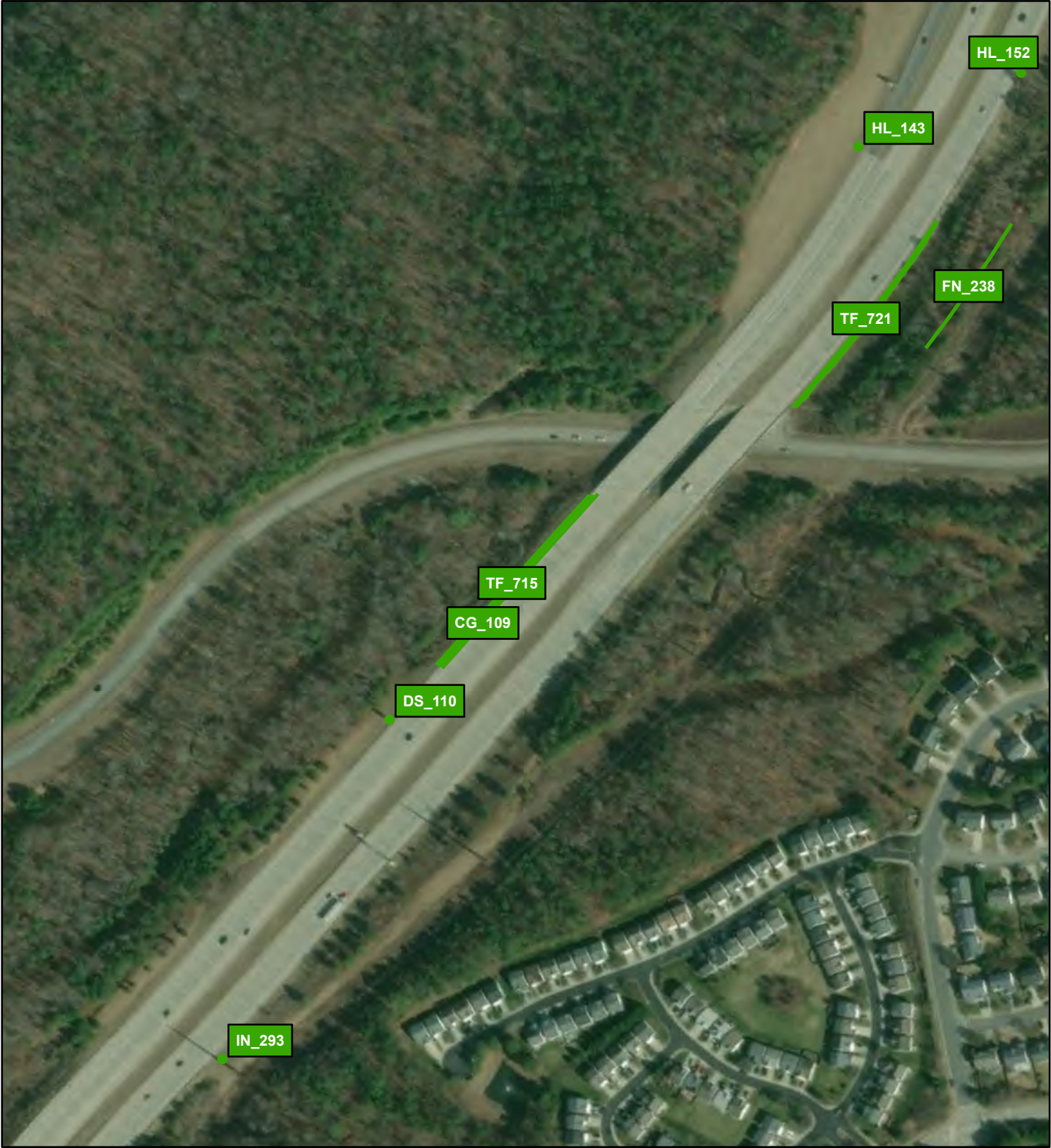
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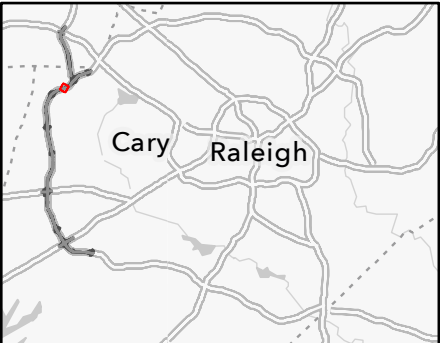
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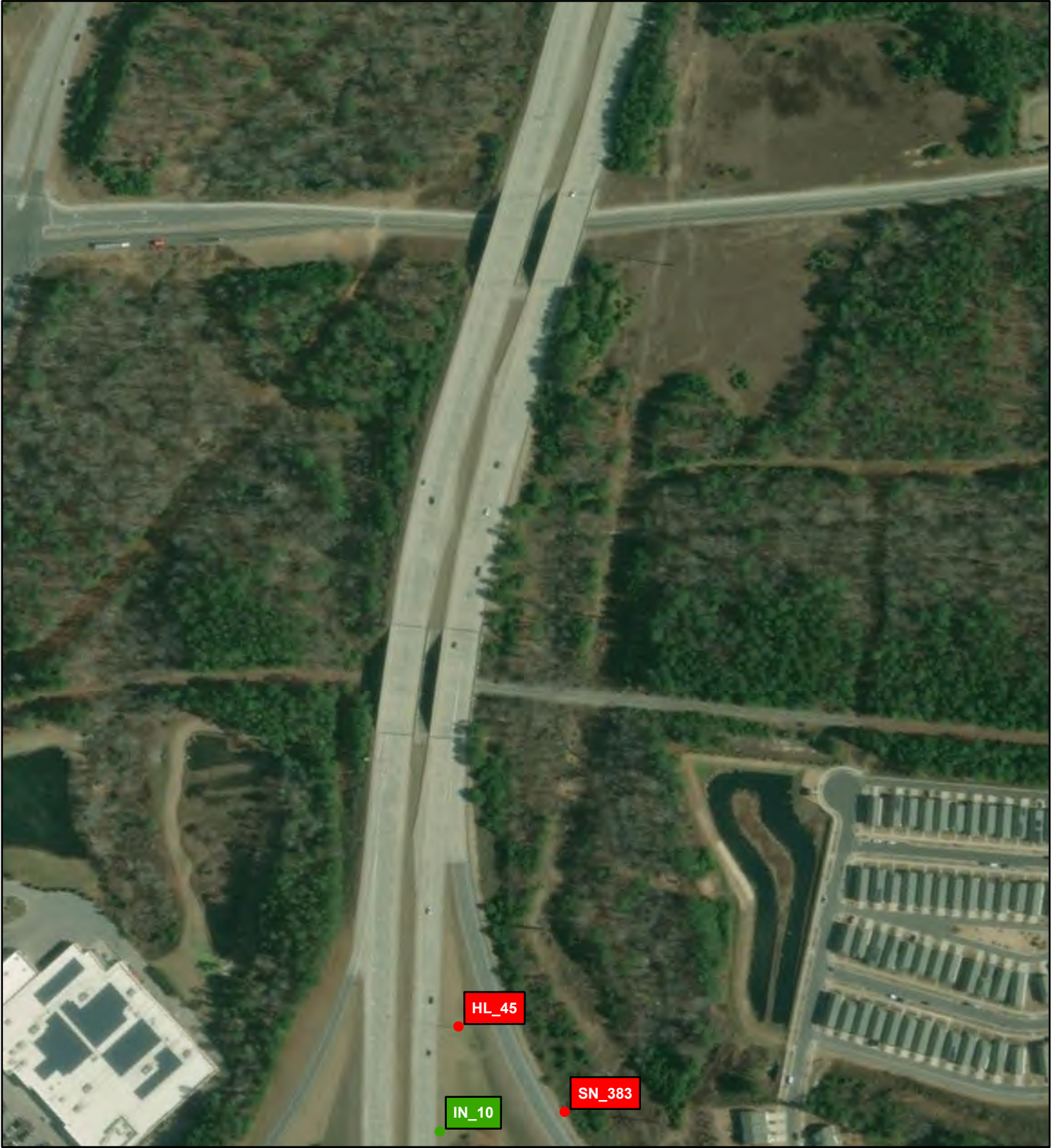
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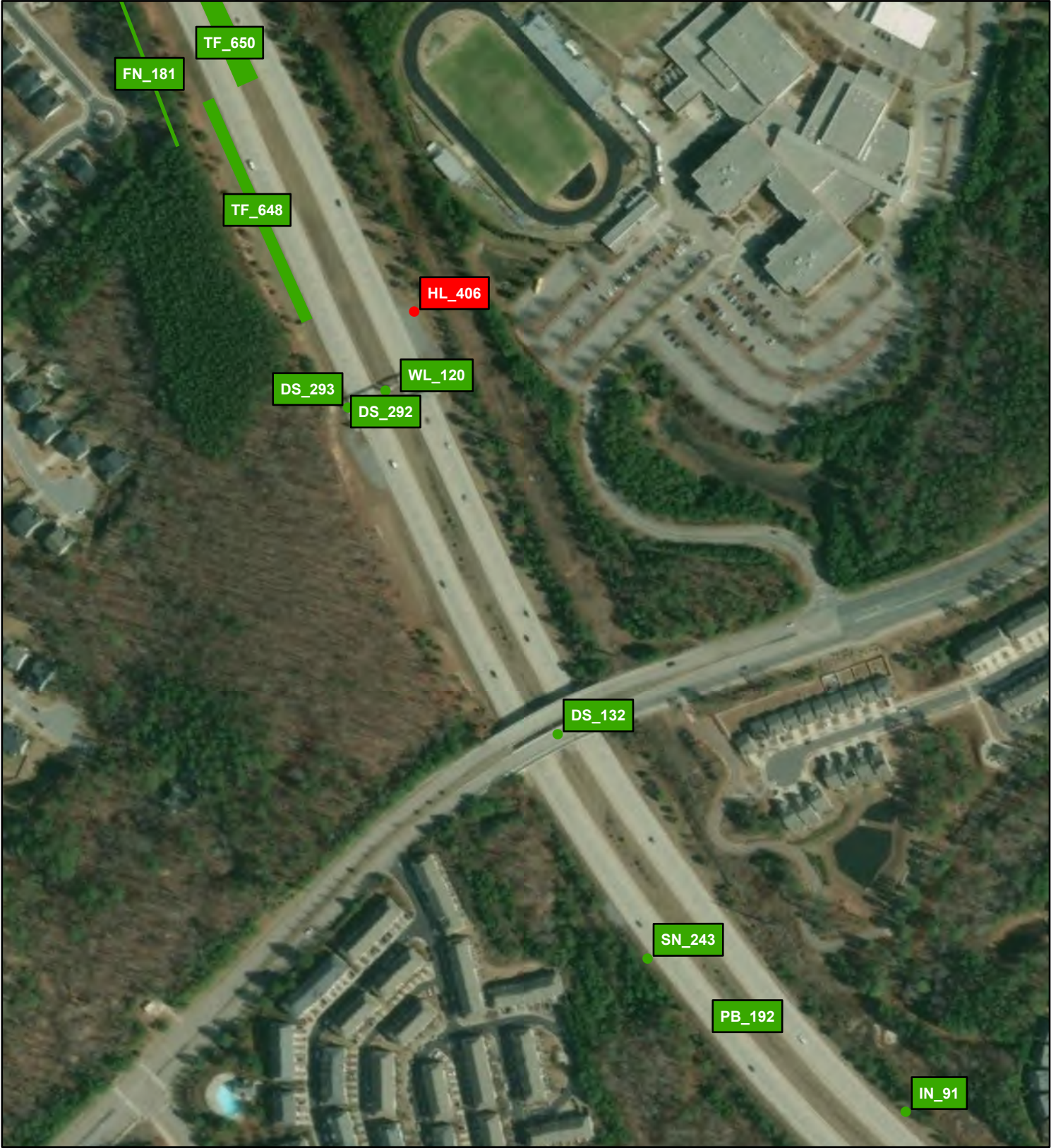
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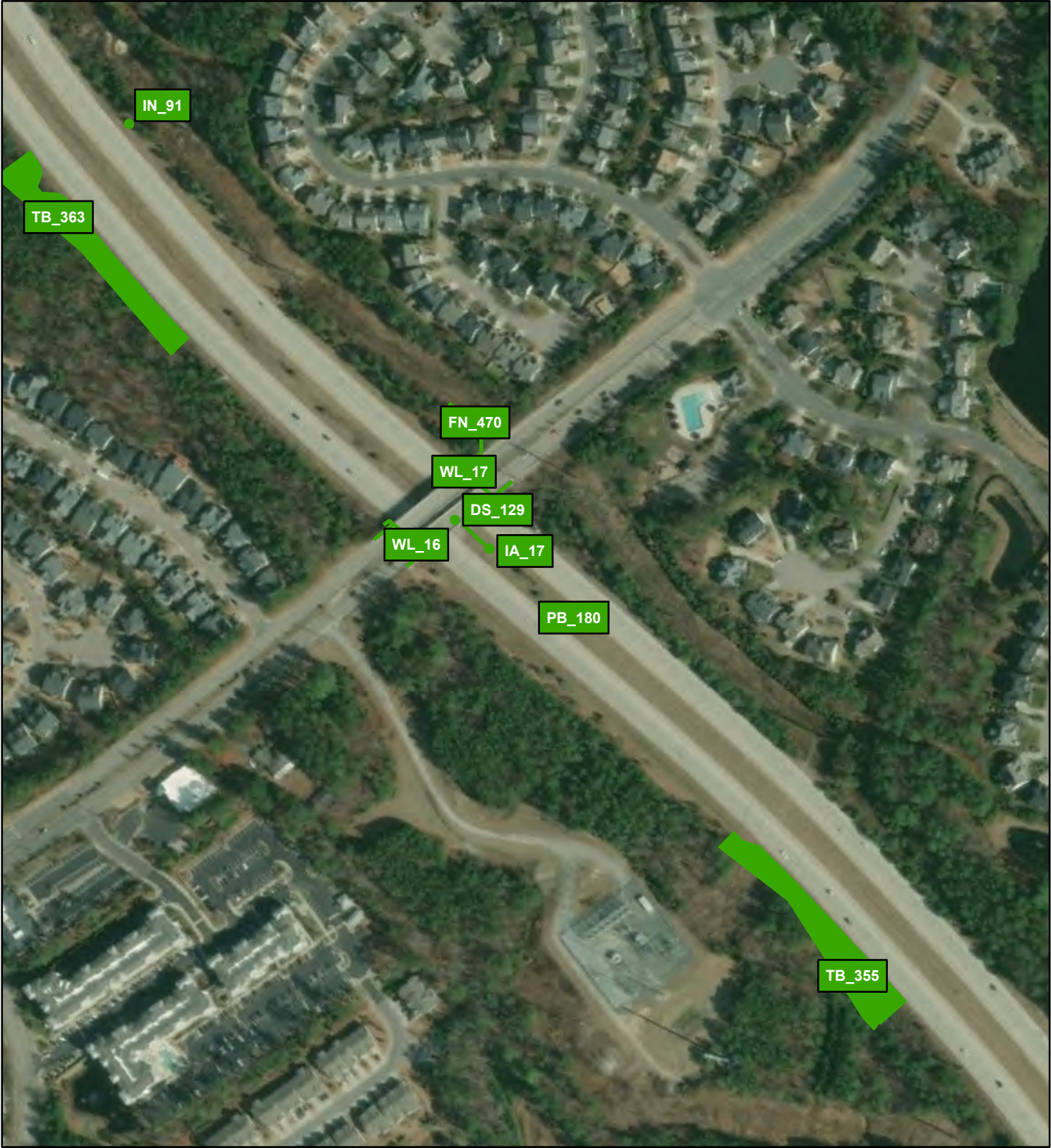
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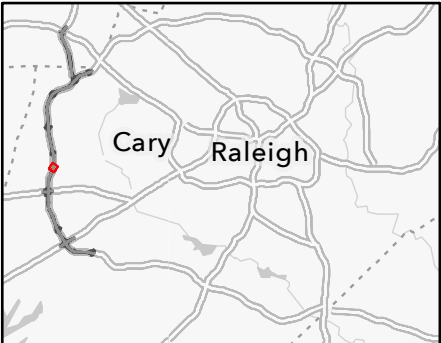
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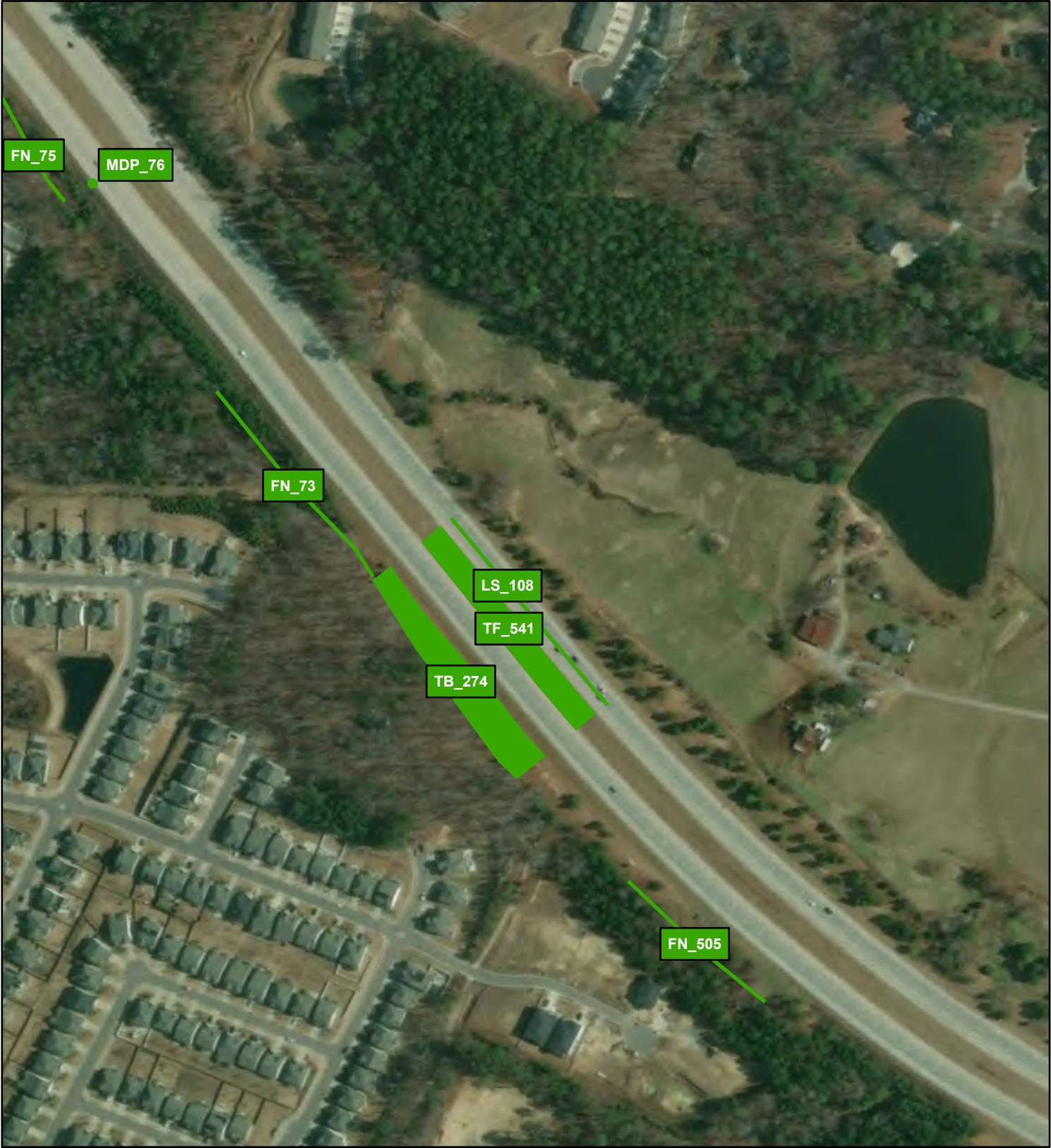
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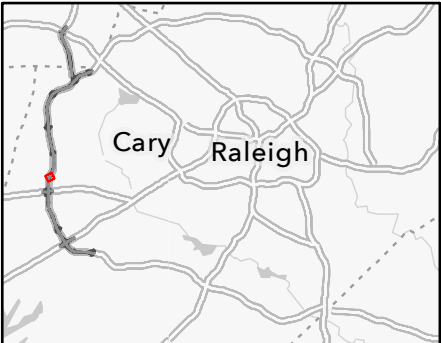
Appendix A: Triangle Expressway 2025 Fourth Quarter Asset Assessment Locations



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Appendix A: Triangle Expressway 2025 Fourth Quarter Asset Assessment Locations



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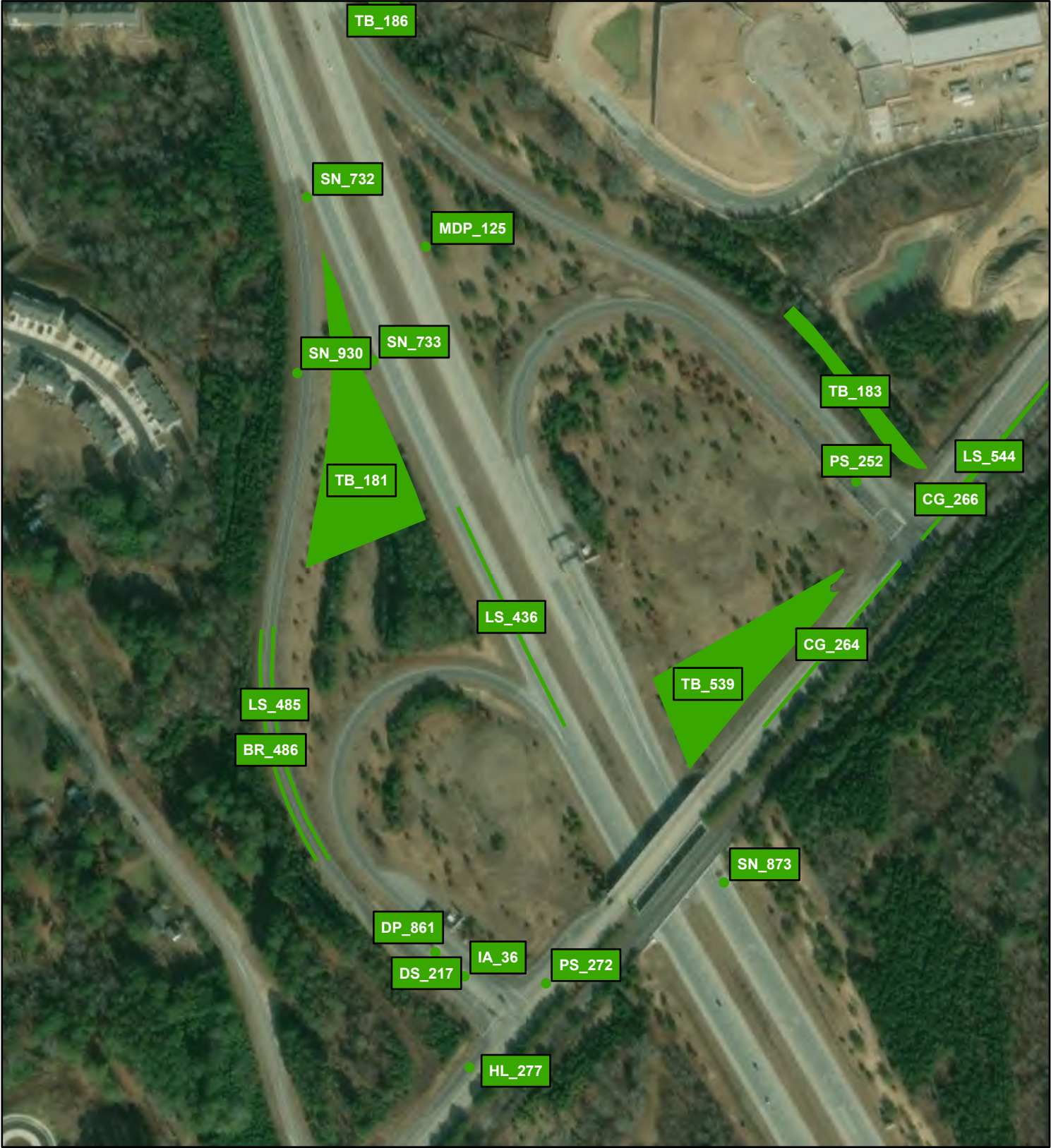
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 NORTH CAROLINA
Turnpike Authority

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Failing Asset



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Turnpike Authority

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NORTH CAROLINA

Turnpike Authority

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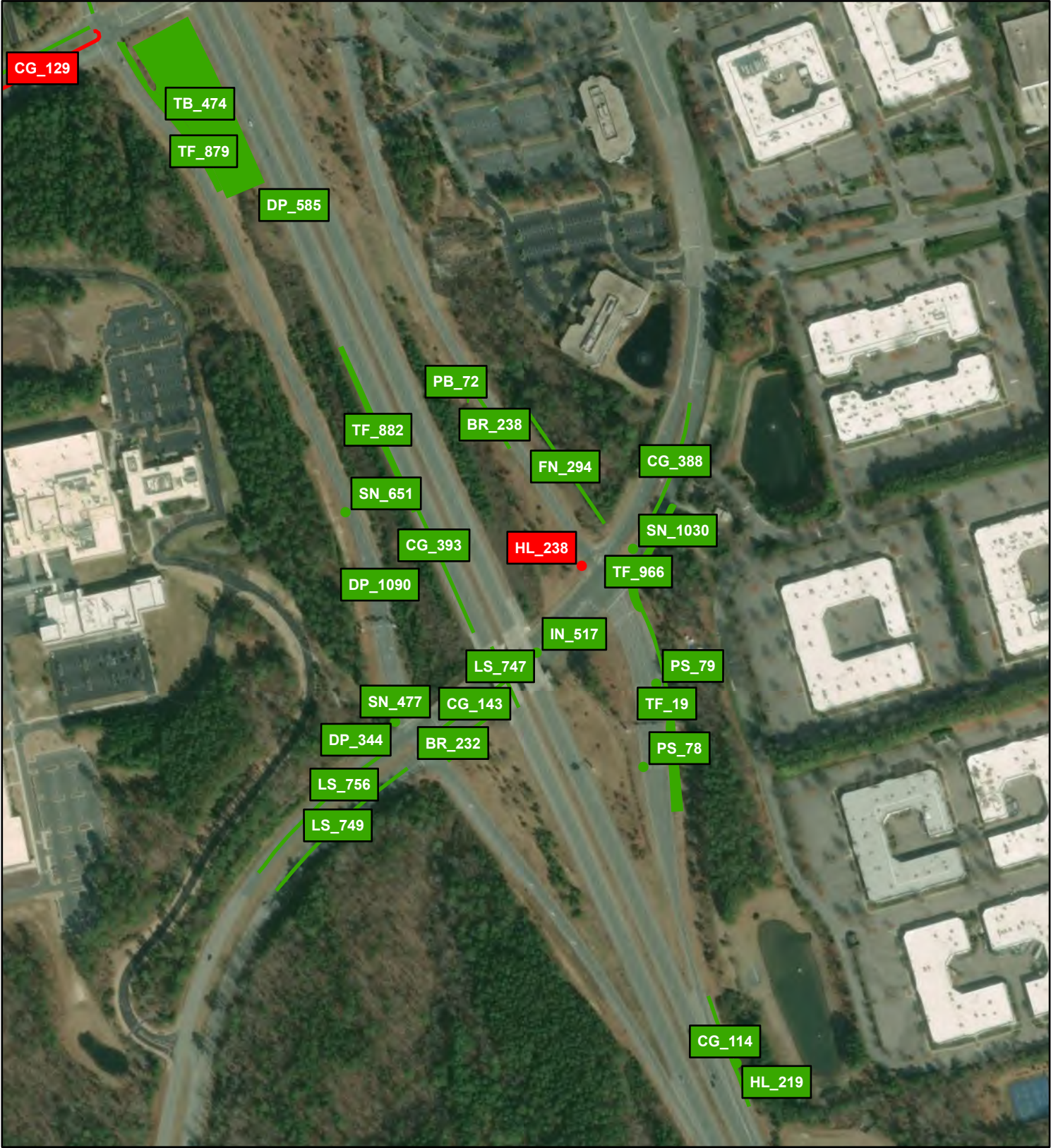
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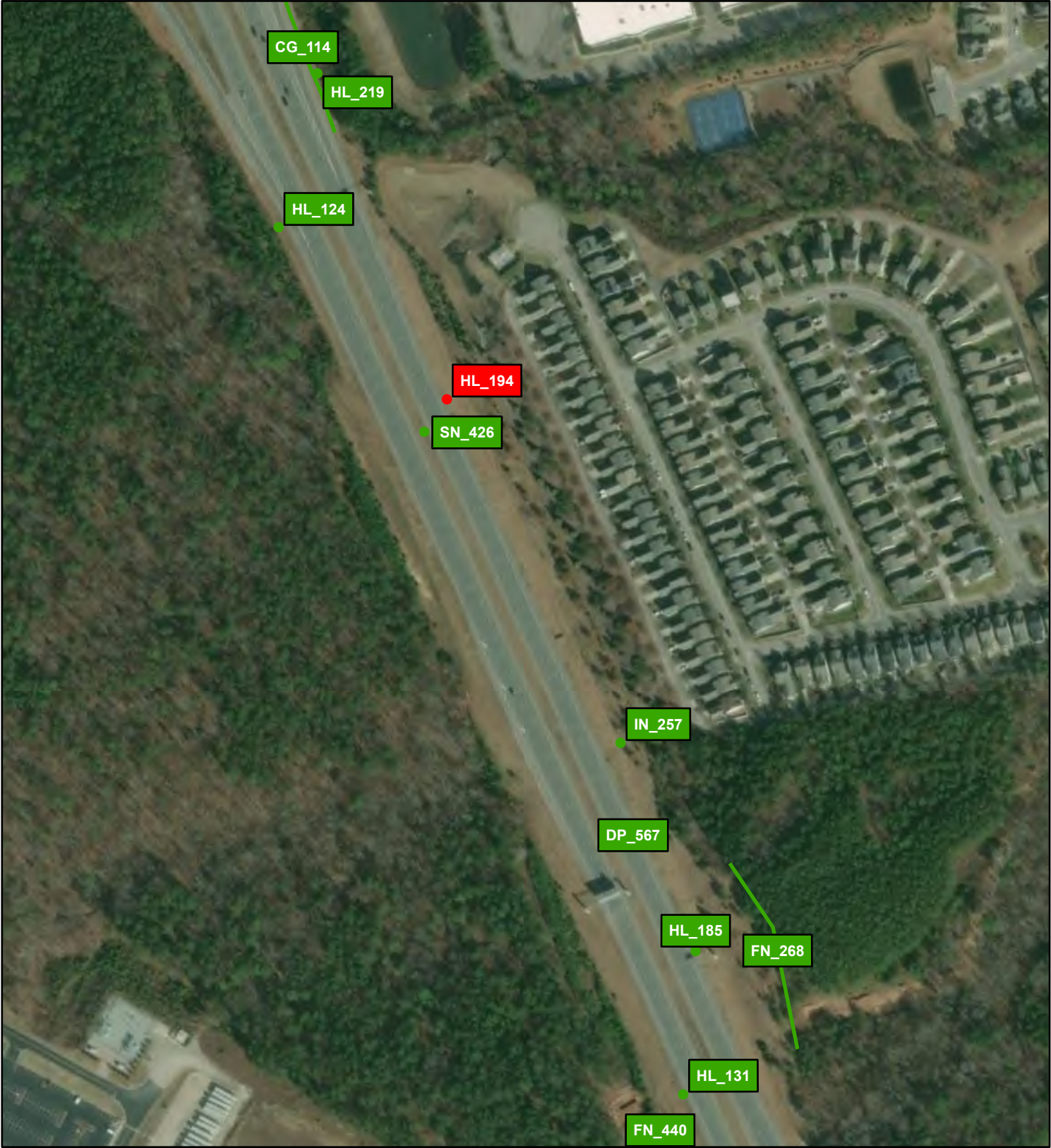
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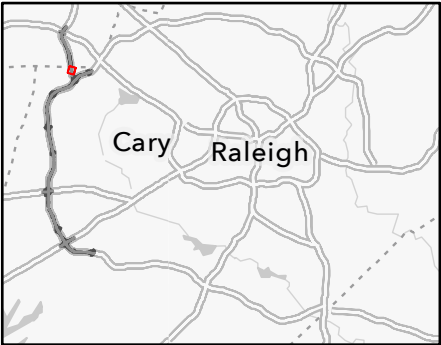
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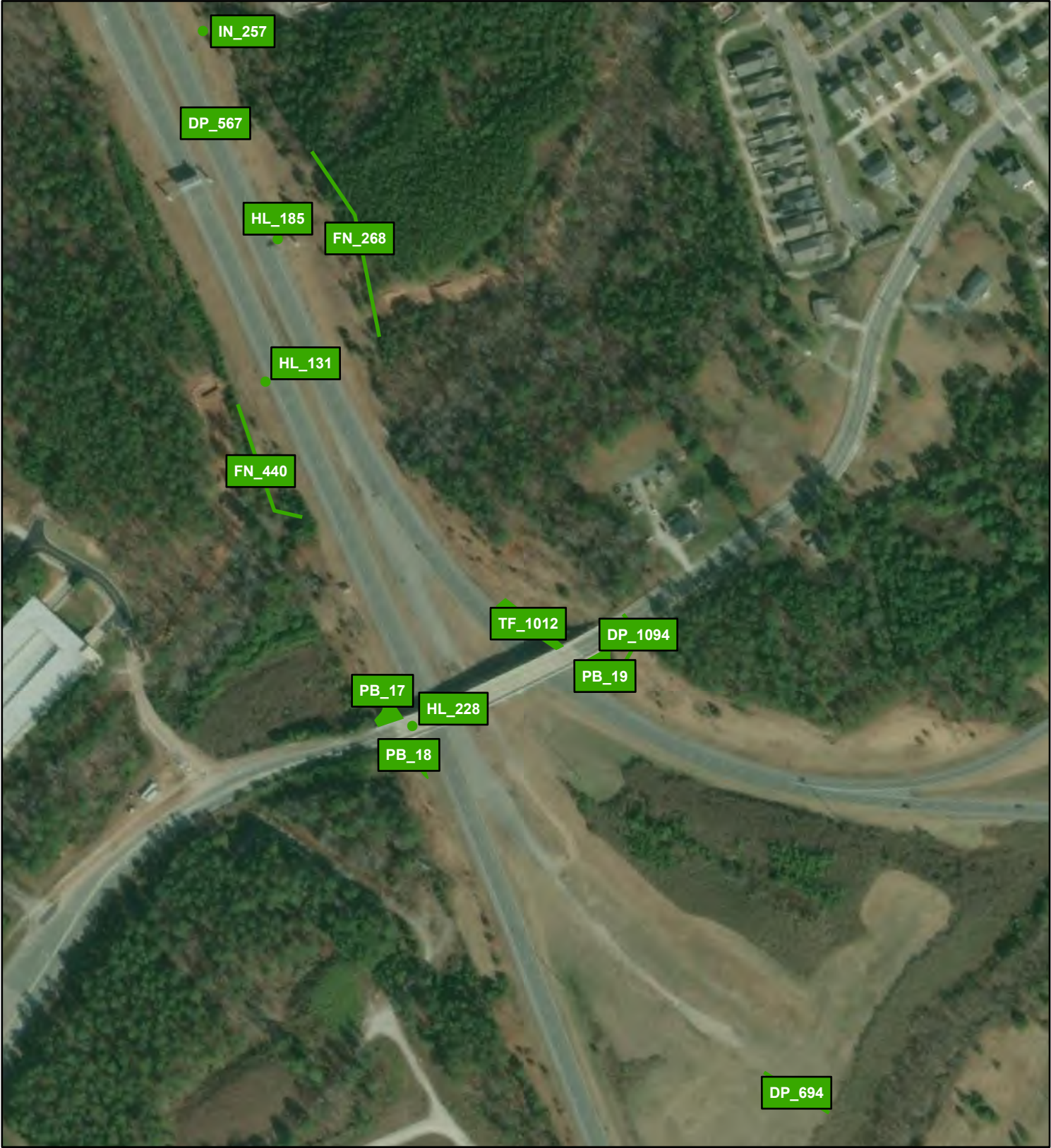
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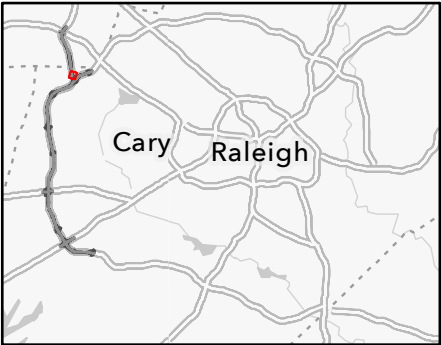
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Appendix A: Triangle Expressway 2025 Fourth Quarter Asset Assessment Locations



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Appendix B

Triangle Expressway 2025 Fourth Quarter Table Results of Assets Failing MRP

Provided below are a series of tables outlining the existing failures that occurred throughout the facility. Assets are defined by an Inventory ID, which is a unique identifier given to each individual asset. The components that make up the Inventory ID are an asset specific prefix along with a number, such as LS_1. The Inventory ID and GIS Reference Page number correspond to the provided map packets and allow for quick location of particular asset failures. Photos of failures were provided when applicable.

All assets and their respective prefixes are listed below:


Guardrail, Concrete Barrier and End Anchors (BR)	B1
Curb and Gutter (CG)	B2
Decorative Supports (DS)	B3
Drainage Pipes (DP).....	B4
Misc. Drainage Structure (MDP)	B5
Fence and Control of Access (FN)	B6
Graffiti (GR).....	B7
Highway Lighting (HL).....	B8
Impact Attenuators (IA).....	B9
Inlets (IN).....	B10
Landscaping (PB).....	B11
Paved Lanes – Asphalt (LS).....	B12
Paved Lanes – Concrete (LS).....	B12
Paved Shoulders (LS).....	B13
Unpaved Shoulders (LS)	B13
Front/Back Slopes (LS).....	B14
Unpaved Lateral and Outfall Ditches (LS)	B14
Litter (LS).....	B15
Roadway Sweeping (LS)	B16
Pavement Striping (LS).....	B17
Pavement Markers (LS)	B18
Delineators (LS).....	B19
Paved Ditches (PD).....	B20
Pavement Words and Symbols (PS).....	B21
Signs (SN)	B22
Tree and Brush (TB).....	B23
Turf Condition (TF)	B24
MSE/Retaining Walls, Sound Barrier Walls, and Screen Walls (WL)	B25

Guardrail, Concrete Barrier, and End Anchors (BR)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Curb and Gutter (CG)



#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Other	CG_129	Settlement		A30

Decorative Supports (DS)


#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Drainage Pipes (DP)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Drain	DP_4	Obstruction		A25
2	Drain	DP_61	Obstruction		A26

Misc. Drainage Structure (MDP)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Shoulder Drain	MDP_43	Obstruction		A11

Fence and Control of Access (FN)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Graffiti (GR)


#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.


Highway Lighting (HL)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Double Roadway	HL_6	Functional Damage (Out at Night)	No Photo Provided	A27
2	High Mast	HL_45	Functional Damage (Out at Night)	No Photo Provided	A5
3	Single Roadway	HL_76	Functional Damage (Out at Night)	No Photo Provided	A13
4	Single Roadway	HL_80	Functional Damage (Out at Night)	No Photo Provided	A13
5	Double Roadway	HL_179	Functional Damage (Out at Night)	No Photo Provided	A2
6	Single Roadway	HL_194	Functional Damage (Out at Night)	No Photo Provided	A32
7	Single Roadway	HL_238	Functional Damage (Out at Night)	No Photo Provided	A31
8	Double Roadway	HL_295	Functional Damage (Out at Night)	No Photo Provided	A24
9	Single Roadway	HL_327	Functional Damage (Out at Night)	No Photo Provided	A17
10	Underpass Lighting	HL_375	Functional Damage (Out at Night)	No Photo Provided	A23
11	Underpass Lighting	HL_377	Functional Damage (Out at Night)	No Photo Provided	A23
12	Single Roadway	HL_406	Functional Damage (Out at Night)	No Photo Provided	A7



Impact Attenuators (IA)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Gating Attenuator	IA_28	Part Damage		A17

Inlets (IN)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Drop Inlet	IN_571	Obstruction		A17

Landscaping (PB)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Plant Bed	PB_9	Health		A20
2	Plant Bed	PB_119	Health		A13

Paved Lanes – Asphalt (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Paved Lanes – Concrete (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Paved Shoulders (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Unpaved Shoulders (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Front/Back Slopes (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Unpaved Lateral and Outfall Ditches (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Litter (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Roadway Sweeping (LS)


#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.


Pavement Striping (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Linear Segment	LS_16	Striping (Line Visibility)		A26

Pavement Markers (LS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Linear Segment	LS_16	Striping (Reflective Markers)		A26

Delineators (LS)



#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Linear Segment	LS_16	Delineators (Reflective - N)		A26

Paved Ditches (PD)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Pavement Words and Symbols (PS)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Left Turn	PS_454	Daytime Assessment / Nighttime Reflectivity	No Photo Available	A6
2	Right Turn	PS_543	Daytime Assessment / Nighttime Reflectivity		A27
3	Thru and Right	PS_582	Daytime Assessment / Nighttime Reflectivity		A26
4	Thru Lane	PS_626	Nighttime Reflectivity	No Photo Available	A13

Signs (SN)

#	Sign Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Exit	SN_383	Missing Parts	No Photo Available	A5

Tree and Brush (TB)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.

Turf Condition (TF)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
1	Turf	TF_677	Bare Ground		A6

MSE/Retaining Walls, Sound Barrier Walls, and Screen Walls (WL)

#	Material Type	Object ID	Failure Type	Photo	GIS Reference Page
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This asset did not produce any failures.